

High Halstow

Design Code

October 2021

Draft

Draft

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URBED (Urbanism Environment Design) Ltd.

a. 5th Floor, 10 Little Lever Street, Manchester, M1 1HR

t. +44 (0)161 200 5500

w. www.urbed.coop



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Introduction

This section introduces the High Halstow Design Code, its purpose and the areas it applies to.

01

1.0 Introduction

Purpose of the Document

High Halstow Parish Council has produced this Design Code to support the High Halstow Neighbourhood Plan policies to ensure that good design is integral to all new developments in the Parish.

This document guides designers and developers to meet the aspirations of the existing High Halstow community and ensure that design proposals are of high quality and integrate well with the existing village and surrounding landscape.

The Design Code sits alongside the High Halstow Parish Neighbourhood Plan, consolidating its vision and design principles to provide specific commitments for development approvals.

Structure of the Document

In January 2021, the Government published The National Model Design Code NMDC (2021), which provides detailed guidance on designing codes, guides and policies to promote successful design.

The High Halstow Design Code follows the National Model Design Code process of understanding the existing context of the area and setting design codes across each of the key themes of:

- Movement;
- Nature;
- Built Form;
- Identity;
- Public Space;
- Use;
- Homes and Buildings;
- Resources; and
- Lifespan

The design codes are listed in a set of tables throughout this document and each code sets a fixed rule

or parameter with an acceptable range for compliance which future development must adhere to.

The NMDC suggests that coding is done at two levels. The first of these includes rules that apply to all development across the Parish and is set out in part 2 of this document.

The second level includes rules that relate to specific areas which the NMDC calls 'Area Types'. These are areas designed to have a particular character and are inspired by the best parts of the surrounding area. This code uses four Area types:

- Village Centre
- Village Fringe
- Rural Edge
- Rural

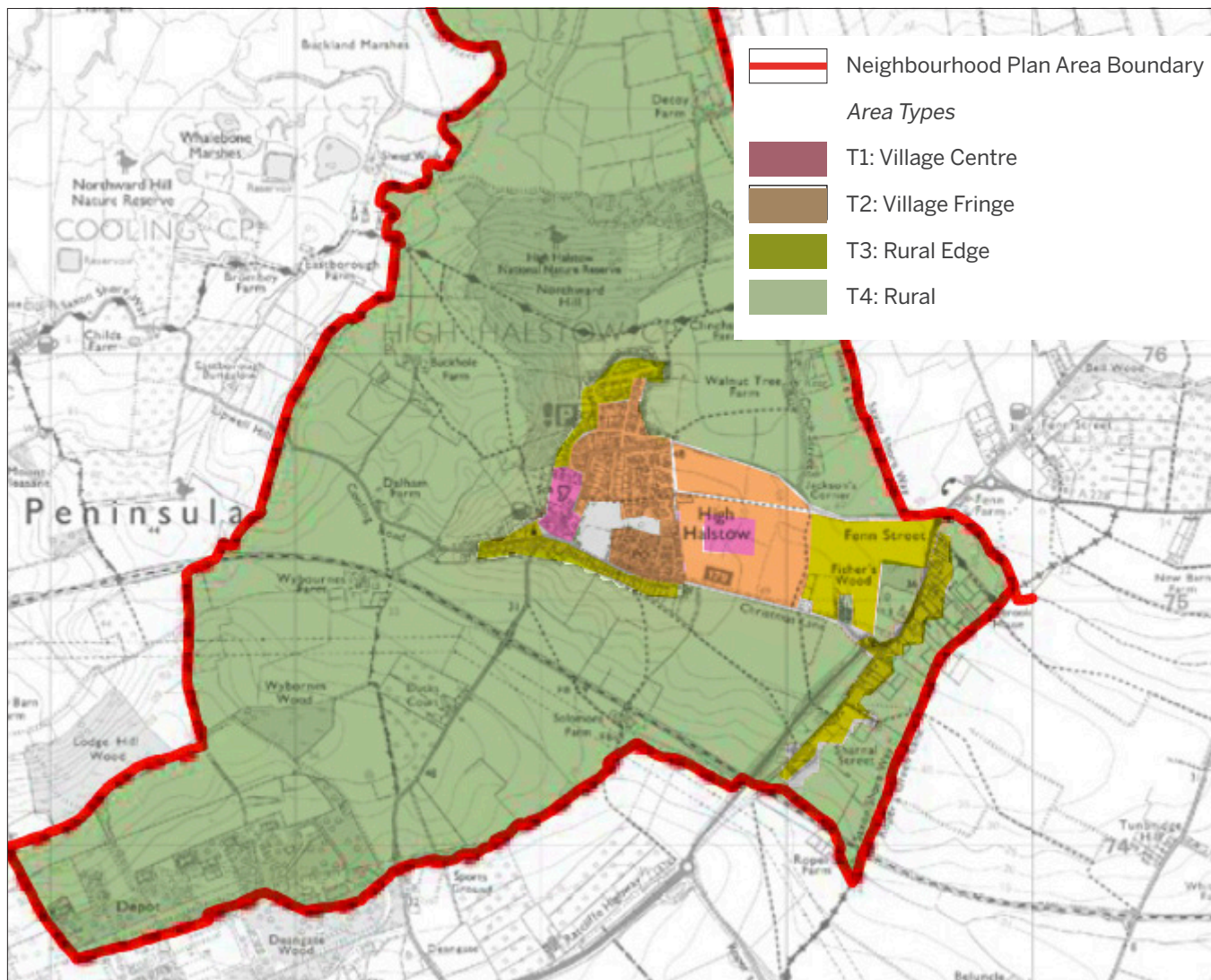
The NMDC also sets out how a framework plan should be prepared for new development sites to show how the rules will be applied. This code therefore also includes a framework plan for the main development site in High Halstow

which has a draft allocation in the local plan for 700 new homes.

Understanding the Context

The guidance in this code is based on national guidance as set out in the National Design Guide and the NMDC Guidance notes. However it is also based on a detailed baseline of the local context in High Halstow, available as a separate document.

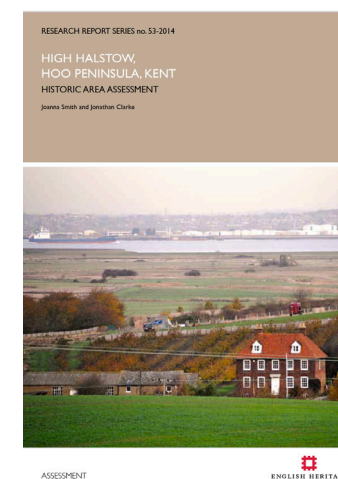
The code has also been informed by the comprehensive historical assessment of the Hoo Peninsula, produced by Historic England in 2014.



Neighbourhood Plan Area & Area Types



High Halstow Neighbourhood Plan, 2020-2037, (2021)



High Halstow Historic Area Assessment, (2014)

1.1 High Halstow

High Halstow Parish Area

The Parish area of High Halstow is located approximately 46km east of London and 8km north of Medway Towns on the northern side of the Hoo Peninsula. The Neighbourhood Plan area covers the extent of the High Halstow Parish Area, shown bounded in red on the adjacent map.

The area sits within the unitary authority of Medway Council and shares its borders with the settlements of St Mary Hoo to the east, Hoo St Werburgh to the south, Cooling to the west and bordered by the Thames River to the north.

The Neighbourhood Plan Parish area includes the village of High Halstow, several hamlets along the A228 road, and many dispersed farmsteads.

The Parish is served by the A228, the Ratcliffe Highway that runs along the spine of the peninsula and passes to the south of High Halstow.

The village is served by Christmas Lane, Britannia Road, Cooling Road and Dux Court Road, all of which are narrow lanes. It currently has limited access to the railway network but there are plans to reopen the goods line running south of the village to passenger services that will include a new station.

There are plans for major housing development on the Hoo Peninsula, including up to 12,000 new homes, much of them in a new settlement at Hoo St. Werburgh that will come to within less than a mile of High Halstow. The council has secured £170M from the Housing Infrastructure Fund HIF to provide the infrastructure needed to serve these new homes.

Land to the East of High Halstow

As part of the plans for the Hoo Peninsula, the land to the East of High Halstow has been identified for 700 new homes by Medway Council in the emerging Local Plan.

This is being considered as part of the masterplanning work being undertaken for Medway Council for the wider HIF-funded growth and is being brought forward by the housebuilder Redrow.

The Parish Council has made clear in the draft Neighbourhood Plan that they oppose the development of this land in principle, and certainly before the infrastructure investment has been made. Additionally, the Parish Council believe that the development of housing in rural areas necessitating large scale infrastructure improvements is inconsistent with climate change concerns.

However, they have also taken a pragmatic stance, recognising that the site is likely to be allocated in the local plan. They have therefore produced this design code to ensure that, if the development comes forward, it is as good as it can be, that it responds to the character and identity of the existing settlement and meets the community's needs.



River Thames

High Halstow Parish

Cliffe

Cooling Parish

Allhallows

St Mary Hoo Parish

High Halstow

Cooling

Cliffe Woods

A228

River Medway

High Halstow Neighbourhood Plan Boundary

Land to the East of High Halstow

Neighbourhood Plan Study Area

High Halstow Parish Area Design Code

This section sets out design codes that apply for new development proposals across the whole Parish area.

02

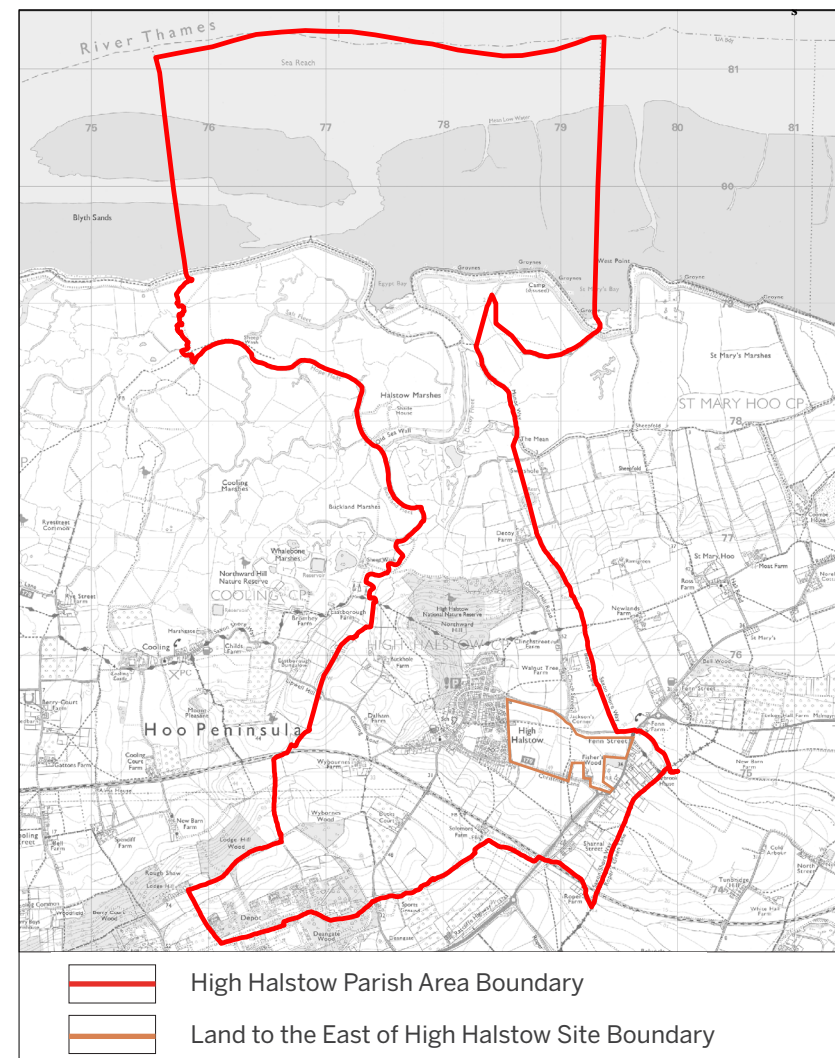
2.0 Parish Design Code

High Halstow Parish Area

Some principles are applicable to all development regardless of which Area Type they are in. The page opposite therefore sets out the rules that apply to all proposed development in the High Halstow Parish area, including the Land to the East of High Halstow as shown on the adjacent plan.

The design codes and their themes align with the following policies in the Neighbourhood Plan:

- Nature and Public Space: The code relates to Policy HH E6: New green spaces and Policy HH E1: Natural Environment;
- Homes and Buildings, Resources and Lifespan: The Code relates to Policy HHE8: Towards zero carbon development;
- Movement, Built Form and Public Space: The code relates to Policy HHM5: Street design, HHM1: Green routes and HHM3: Bus services, routes and infrastructure;
- Built Form, Identity, Use, Homes and Buildings, Resources and Lifespan: The Code relates to Policy HHPQ1: Design;
- Uses: The Code relates to Policy HHC1: Village Centre and facilities and Policy HHC2: Education.



High Halstow Parish Area

Movement

Objective: That all new development should create a walkable, safe network of streets that discourage car use and encourages walking and cycling:

M01 - Streets:

New streets should link at either end to other streets and allow free access for walking and cycling. Car access may be restricted within the street network so that not all streets allow through traffic.

M02 - Street Hierarchy:

All streets should be allocated to a level of the street hierarchy. Each street should follow the appropriate design guidance in section 3 of this code.

- High Streets should provide for through traffic and local shops and services.
- Secondary Streets should give access into each part of the neighbourhood
- Tertiary Streets should allow only for frontage access and may include home zones.

M03 - Speed Restrictions:

All new streets should be designed for a 20mph speed limit.

M04 - Public Transport:

All new development should be within a 5-minute walk of a bus stop with a service of at least four buses an hour.

M05 - Active Travel:

New development should incorporate clear cycle and footpath routes and contribute to the improvement of cycle and footpath routes in the wider area, particularly to the station. These could be wide, well surfaced, safe and well lit. All new homes should make provision for at least 2 cycle parking spaces, including apartments.

M06 - Parking

Parking provision should be in line with the local plan standard. On High Streets and Secondary Streets on-plot parking should not be in front of the building.

M07 – Servicing:

New development should integrate the requirements of utility providers, refuse collection and emergency access without compromising the quality of place by obstruction of movement or visual intrusion.

Nature

Objective: Development should enhance the natural environment by creating a network of green spaces and enhancing biodiversity and the wellbeing of residents.

N01- Open Space Provision:

Development of a scale that triggers the provision of new green space should be provided in line with standards established by Medway Council and should provide at least 1.6ha of open space per 1000 people and should include a proportion of allotments.

N02 - Play Provision:

All homes should be within 100m of a local area of play, 400m of a local equipped area of the plan and 700m of a Neighbourhood equipped area of play (see national Model Design Code Guidance notes for definitions)

N03 - Open Space Design:

Spaces should be designed to be overseen from surrounding buildings, streets and public spaces - open spaces should never be enclosed by back gardens. All new open spaces should be designed in line with the guidance in N.1.iii of the NMDC Guidance Notes.

N04 - Drainage:

Schemes should incorporate Sustainable drainage systems that achieve greenfield run-off rates while enhancing biodiversity in line with the guidance in N.2.ii of the NMDC Guidance Notes.

N05 - Biodiversity:

Schemes should achieve a 10% increase in biodiversity as measured by using the Natural England Biodiversity Metric 3.0. Schemes should be designed to enhance biodiversity, including the retention of existing trees, hedges and habitats, the reduction of light pollution and the creation of new habitats and ecological networks.

N06 - Street Trees:

All new streets should incorporate street trees within the public realm at spacings of at least one tree every 24m.

Draft Built Form

Objective: To create a characterful village environment with well-proportioned streets and public spaces.

B01 - Public and Private:

New development must create a clear separation between the public fronts of houses that face onto streets and the private rear gardens.

B02 - Built Form:

All new homes should follow the density guidance, building line, and heights for the Area Types set out in section 3

Identity

Objective: All schemes should be designed to respect and enhance the character of High Halstow and the villages of the Hoo Peninsula.

I01 - Sense of Place:

All schemes should be designed to enhance local character and legibility by making use of local materials and detailing

I02 – Masterplan:

New development should be guided by a masterplan that provides a long term framework for the entire built-out period. The masterplan should be designed to create a sense of plan and identity, including the identification of views and vistas, landmarks and corner features. See para I.1.iii of the NMDC Guidance Notes.

I03 - The Identity of Buildings:

All buildings should take account of the principles set out in Para I.2 of the NMDC Guidance Notes.

I04 - Public art:

All new schemes should include a strategy for the provision of public art.

Public Space

Objective: Development should create and enhance an attractive, safe and inclusive network of public spaces, including streets, squares and green space.

P01 - Streets:

These should be designed as public spaces capable of accommodating a range of uses (rather than just the car).

P02 - Street Design:

The design of each street should relate to its place in the street hierarchy and the Area Type.

P03 - Home Zones:

Tertiary level streets should be designed using home zone principles as set out in Para P.2.iii of the NMDC Guidance Notes.

P04 - Safety:

All streets, squares and green spaces must be fronted onto and overlooked by development. Streets and public spaces should be designed for natural surveillance from other street users and surrounding buildings. Secured by Design principles as set out in Para P.3.i of the MBDC Guidance Notes should be followed.

Use

Objective: To create a diverse, neighbourhood with a mix of people and local services and facilities.

U01 - Housing:

New schemes should include a mix of housing in terms of type and tenure. They should meet Medway's affordable housing standard and include a diversity of house types in terms of form (detached, semi-detached and terraced, apartments, bungalows, elderly persons accommodation, and supported/assisted living accommodation) and size (number of bedrooms).

U02- School:

The school should be integrated into the village relating to both the existing and new development and catering for joint use of its facilities.

U03 - Shops:

The new village centre should include at least five shop units at least one of which is a café, let on terms that encourage their occupation by local businesses and provides active frontages.

Homes

U04 - Community Facilities:

New development should enhance and expand the provision of local services such as doctors surgeries, community space and childcare.

U05 - Homeworking:

All new homes should be designed to facilitate home working in terms of space, broadband connectivity and homeworking hubs.

U06 – Self-build:

Opportunities should be found to incorporate self-build plots into new development in line with Medway's emerging Local Plan policy that supports allocation of self-build plots in new development.

Objective: All new housing will be well designed and suitable to the needs of its occupants.

HB01 - Housing Design:

The Nationally Described Space Standards will apply to all new housing.

HB02 - Daylight:

Housing should be designed to maximise internal daylight, while avoiding overheating, and all homes should be dual aspect.

HB03 - Privacy Distances:

A minimum privacy distance of 17m will apply between windows to the rear of properties. There will be no privacy distance at the front of a property.

HB04 - Gardens:

All new homes should have access to external amenity space. For houses, this should be at least as large as the footprint of the house.

Resources

Objective: New development should be zero carbon ready and facilitate a sustainable future for the village as a whole.

R01 - Zero Carbon:

All new houses should be zero carbon ready in terms of day-to-day resource use. This will mean following the 2025 Future Homes Standard, including fabric efficiency and no-fossil fuel-based heating and hot water systems, even if building starts before 2025.

R02 - Passive Design:

Housing design should take account of orientation. The internal arrangement and glazing ratio of homes should depend on the orientation of the home.

R03 - Embodied Energy:

New homes should include an assessment of embodied energy and include a plan to reduce it by 30% compared to a typical new build dwelling.

R04 - BREEAM Rating:

All new buildings should achieve at least a BREEAM 'Excellent' Rating.

R05 - Water Usage:

All new homes should be designed to achieve water usage of 110 litres per person per day.

Lifespan

Objective: To ensure that all new development is well managed and incorporates the views of the new and existing community.

L01 - Management Plan:

All new development should include a management plan setting out adoption arrangements and management arrangements for non-adopted areas and facilities, including the potential for community management.

L02 - Community Participation:

Separate consultation will be required with the community on all schemes throughout the development process up to and including management.

2.1 Land to the East of High Halstow

The Parish Council maintains that development of the Land East of High Halstow is unsustainable, especially if brought forward before the new Medway Local Plan (2019 to 2037) is adopted and before appropriate infrastructure improvements to the site and surrounding areas are made.

The Parish Council acknowledges, however that the site could come forward within the new Local Plan and if so, want to ensure that development is brought forward to the highest standards and quality and will meet the existing and future needs of High Halstow Village.

Accordingly, the Parish Council have produced a Masterplan Framework and specific rules for development of Land East of High Halstow.

Land East of High Halstow Masterplan Framework

In addition to the general rules set out in the following section, the code includes specific rules for different character areas - called Area Types in the NMDC.

These are set out on the following pages and can be applied to the existing parts of the village. However, their main role is to set detailed rules for the site known as the land East of High Halstow to the South of Britannia Way and North of Christmas Lane.

In order to apply these rules it is necessary to have a framework plan to show how they apply to the new development. The framework plan opposite has been designed to do this.

This has been developed in discussion with the developers for the site and their masterplanners to produce a plan that all parties can agree.

Our understanding is that this is the case except for point 8 described below:

Key Design Principles

- 01 Development that integrates into and relates to the existing Village of High Halstow retaining its identity as a rural settlement;
- 02 A new network of streets with a clear street hierarchy which connects with the existing settlement via Britannia Road and Christmas Lane;
- 03 A bus route connecting the existing settlement village centre with the new proposed village centre and down to the proposed new railway station to the south of the Parish;
- 04 A new village centre, with a new primary school and a mix of shops and community uses;
- 05 The creation of three Area Types that vary in built-form, street typology, densities and identity;
- 06 Provision of new open parkland space, integrating SuDs and new tree planting;
- 07 A substantive buffer between High Halstow and the Ratcliffe Highway with extensive tree planting and enhancement of the landscape character;
- 08 A new main access road to the village centre direct from the roundabout on the Radcliffe Highway. This potentially allows part of Christmas Lane to be closed to car traffic. This will create a strong route to the proposed new station;
- 09 Provision of green spaces, designed to be actively used, that follow desire lines and provide for a range of recreational uses, including opportunities for play and growing spaces;

- 10 Ensure maximum connectivity and permeability between the two village centres and improvements to the movement network that provides an opportunity for people to walk and cycle across the site safely;
- 11 Safeguards and enhances the existing woodland to the east of the site and provides new tree planting; and
- 12 New street tree planting on all new primary and secondary streets.



Framework Plan

DRAFT

Area Types within High Halstow Village

| | |
|-------------|----------------|
| Dark Blue | Village Centre |
| Medium Blue | Village Fringe |
| Light Blue | Rural Edge |



DRAFT

Area Types Design Code

This section sets out design codes that apply for within four different area types across within the Parish area.

03

3.0 Area Type 1: Village Centre

Location

The Village Centre Area Type relates to both the new and existing village centres. The aim is to create twin centres for the village, the old centre with the pub, church and community halls and the new village with the new school, shops and other facilities.

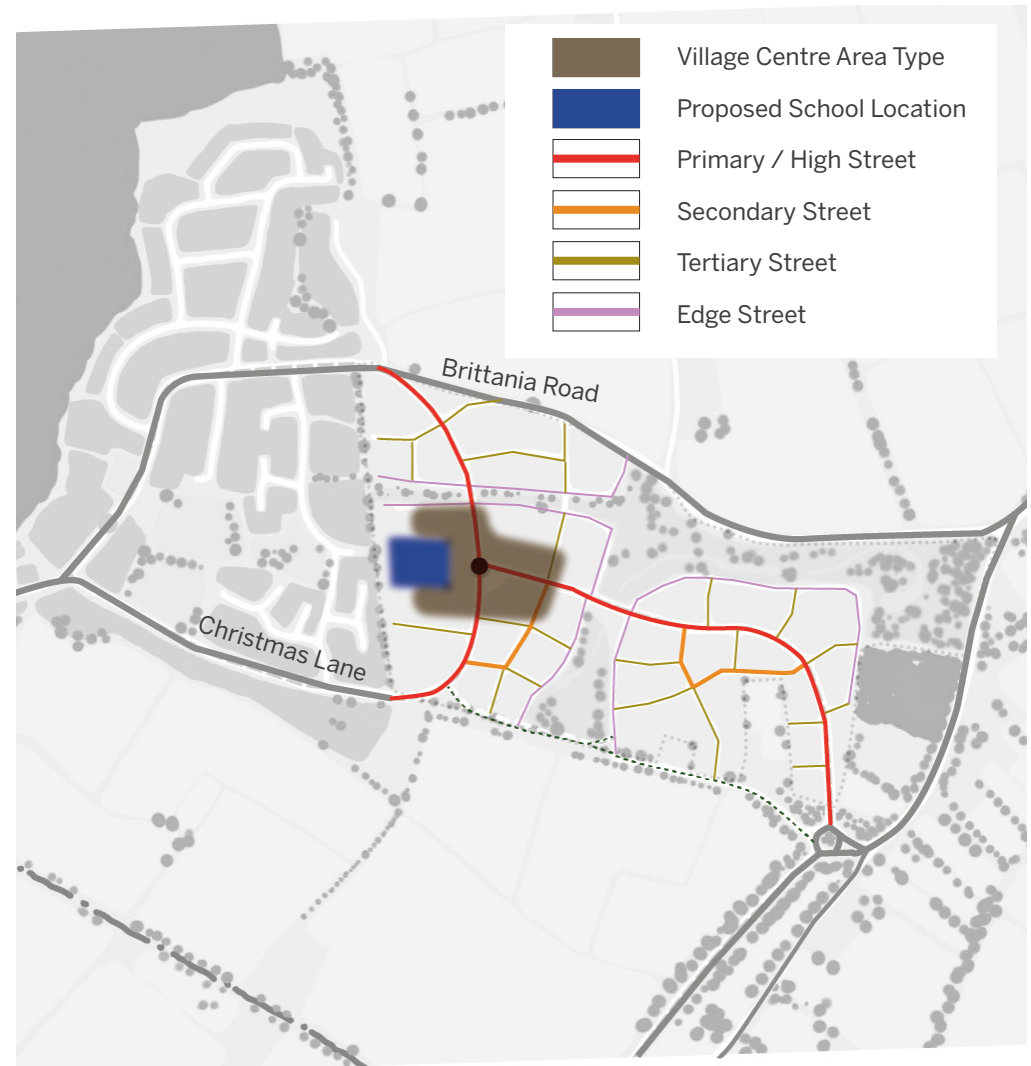
The new village centre is at the junction of two proposed high streets and will be built around a public square bounded by the school and a set of local shops and services.

Character

The Village Centre Area Type is based on an analysis of the centre of other nearby villages like Cliffe. The centre of the village covers a small area but is more urban in character with the following characteristics:

- Housing densities of between 50-60 dwellings per hectare.

- Predominant heights of 2 storeys but with some three-storey buildings.
- Buildings that generally join to their neighbours creating terraces.
- Buildings that closely follow a clearly defined building line.
- Buildings that are set back no more than a meter from the back of pavement.
- Mixed-use development with apartments or offices over ground floor retailing, cafes or other facilities.
- Distinctive buildings and public art that mark the centre of the village and create landmarks seen along the approaching roads.
- High quality public realm that explore a shared surface treatment on the main square.



Village Centre Area Location

Village Centre Coding

The following design codes set out the regulatory rules for the Village Centre area, which development proposals must adhere to.

The subsequent pages present illustrations, sections and precedents that show how the design codes create the character of the Village Centre.

VC 1 - Streets:

Development in the village centre will define the primary, secondary and tertiary streets shown on the plan on page 21. The high street will make provision of a bus service and bus stop in the village square.

VC2 - Cycle Parking:

To be provided based on 2 spaces per dwelling and 3 visitor spaces per shop plus additional spaces for the school. The residential provision is to be provided in courtyards while the visitor cycle parking should be on-street.

VC3 - Car Parking:

To be provided based on 1 space/ residential unit with 25% additional visitor parking. This parking will be provided on-street and in rear courtyards.

VC4 – Green spaces:

The main open spaces (the new square and the school grounds) should be designed to maximise biodiversity.

VC5 – Housing Density:

New housing should be built at net densities of 50-60 dwellings / hectare

VC6 – Party Wall:

Most buildings (75%) should be joined to their neighbours, creating a party wall.

VC7 - Form:

Development should create a varied and fine-grained development form with an informal layout, a variety of building heights, narrow plots and a range of materials.

VC8 - Building Line:

A clear building line should be set for development. This should be set no more than 1m from the back of the pavement. Buildings should project forward and back from the building line by no more than 0.5m.

VC9 - Height:

The eaves height of new buildings should not exceed 10m or be less than 6m. The total height of buildings should be no more than 3m above the eaves height and most buildings will have pitched roofs.

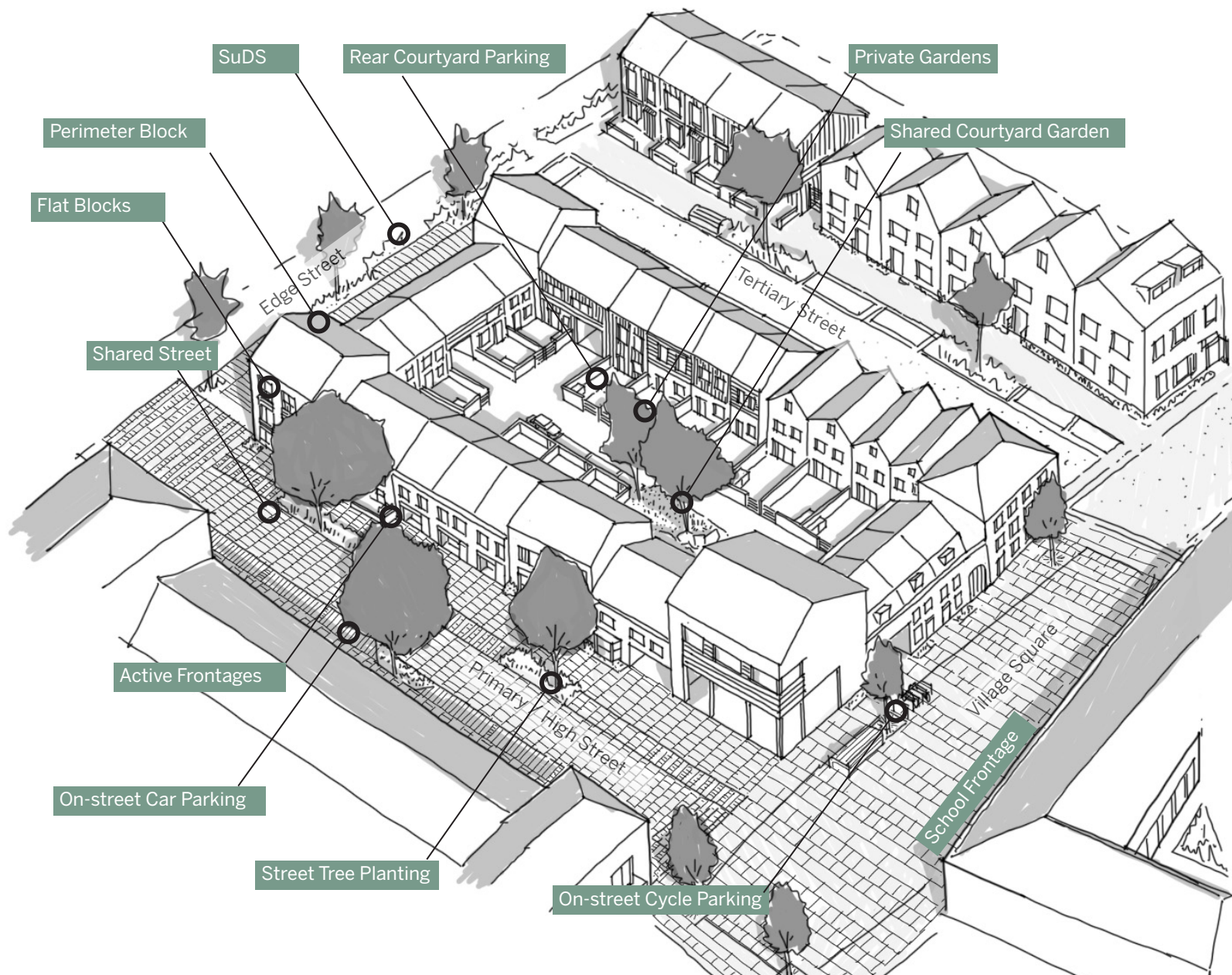
VC10 - Boundary Treatment:

Where small front gardens are created the boundary treatment should be a dwarf wall.

VC11 - Identity:

Buildings should be designed as a contemporary interpretation of local vernacular:

- Entrances should be marked with porches.
- Ground floors should be differentiated either architecturally or by materials.
- Rooflines should be varied with some gables facing the street.
- A variety of materials should be used, including red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.
- Vertical windows with bay windows.



VC12 - Public Space:

New public spaces should be designed to the specifications set out in the adjacent table.

VC13 - Mix of uses:

The high street and square should incorporate at least 5 shop units. These will create an active frontage on at least 50% of the frontage.

VC14 - School:

The new school will be subject to these rules; it should follow the building line, face the square and take its main pedestrian access from it.

| VC12 - Public Space | | | | | | | | | |
|---------------------|-----------------|----------|-----------------|--------------------------|--------------------|-------------------|-----------------------------|----------------------|--------------------|
| | Enclosure ratio | Width | Active Frontage | Building line compliance | Parking | Cycling | Footway | Street Trees | Tree Spacing |
| High Street | 1:1.5 | 13-16.5m | 50% | 90% | One side of street | On shared surface | At least 2m/ shared surface | One side alternating | Min 12m Max 24m |
| Secondary Street | 1:2 | 11-13m | NA | 80% | Both sides | On Carriageway | At least 2m | Both sides | Min 12m Max 24m |
| Tertiary Street | 1:2 | 10-12 | NA | 70% | Both sides | On Carriageway | Part of shared surface | Both sides | Min 12m Max 24m |
| Parkside Street | NA | NA | NA | 70% | One side of street | On Carriageway | On shared surface | One side | Min 12m Max 24m |

Village Centre
Village Square



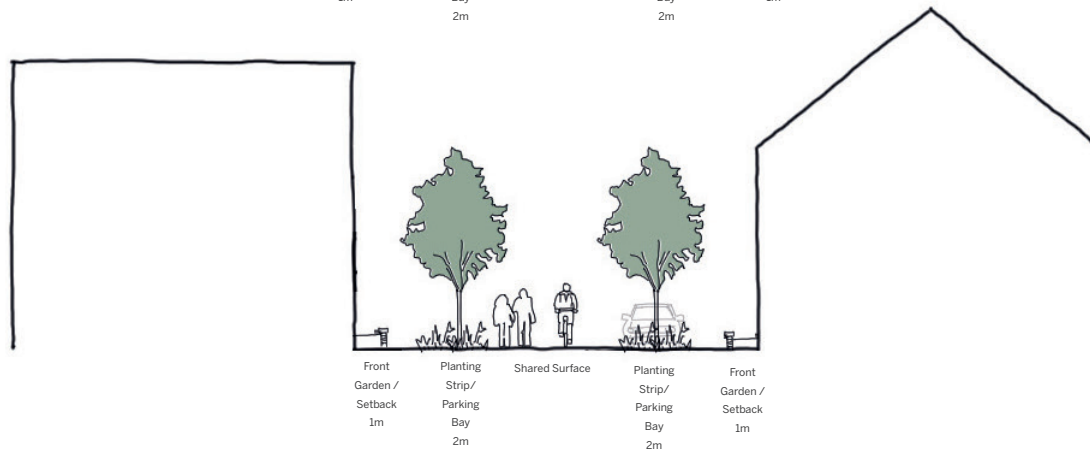
Village Centre
High Street



Village Centre
Secondary Street



Village Centre
Tertiary Street



Cliffe, Reed Street - terraces, short frontages and sense of enclosure to the street



Precedent - Village Square in Lightmoor Village, Telford



Precedent - Urban Village Shared Street

3.1 Area Type 2: Village Fringe

Location

The Village Fringe Area Type includes the area around the new village centre next to the existing village. It also includes most of the existing village.

The area includes high streets, secondary, tertiary and parkside streets, although the former will not include any shops.

Character

The character of the area is drawn, in part, from the existing development in High Halstow and other local villages. However, the aim of Area Types is to create a vision for how they should be in the future, rather than just replicating what exists.

The Village fringe will therefore include:

- Housing densities of between 35 and 45 dwellings per hectare.
- Predominant heights of 2 storeys.
- Buildings that are generally semi-detached and have short terraces and bungalows.
- Buildings that informally follow a building line.
- Buildings that are set back 2-5m from the back of pavement.
- Solely housing development but with provision for home working.
- Housing design that is a contemporary interpretation of the local vernacular.



Village Fringe Area Location

Village Fringe Coding

The following design codes set out the regulatory rules for the Village Fringe area, which development proposals must adhere to.

The subsequent pages present illustrations, sections and precedents that show how the design codes create the character of the Village Fringe.

VF1 - Streets:

Development in the village fringe will define the Primary, secondary, tertiary and parkside streets shown on the plan on page 27. The high street will make provision of a bus service.

VF2 - Cycle Parking:

To be provided based on 2 spaces per dwelling to be provided on-plot.

VF3 - Car Parking:

To be provided based on 1.5 spaces/residential unit. One unit should be provided on-plot with the remaining third being unallocated on-street parking. No more than 40% of the on-plot parking should be in front of the building line.

VF4 – Green spaces:

The garden and courtyard spaces should be designed to maximise biodiversity.

VF5 – Housing Density:

New housing should be built at net densities of 35-45 dwellings / hectare

VF6 – Party Wall:

Housing should be joined to their neighbours on at least one side, creating a party wall.

VF7 - Form:

Development should create a varied and fine-grained development form with an informal layout, narrow plots and a range of materials.

VF8 - Building Line:

A clear building line should be set for development. This should be set 2-5m from the back of the pavement. Buildings should project forward and back from the building line by no more than 1m.

VF9 - Height:

The prominent building height should be 2 storeys with an eaves height not exceeding 7m. Three storey buildings, however, permitted with a maximum eaves height of 8m (accommodation partly in the roof). The total height of buildings should be no more than 3m above the eaves height, and most buildings will have pitched roofs.

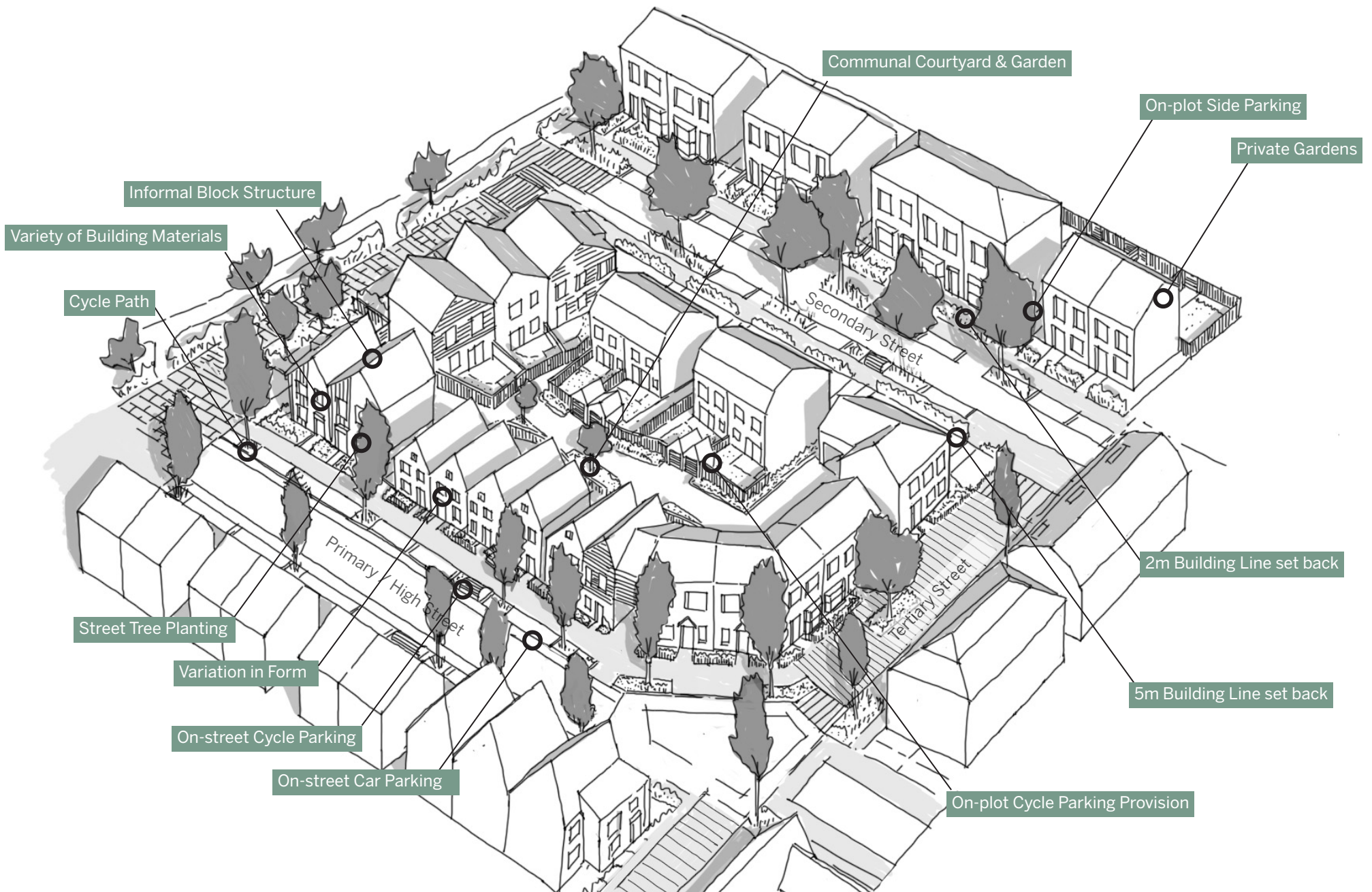
VF10 - Boundary Treatment:

The boundary of front gardens should be marked by a wall, hedge, or fence no more than 1m in height.

VF11 - Identity:

Buildings should be designed as a contemporary interpretation of local vernacular:

- Entrances should be marked with porches.
- Rooflines should be varied with some gables facing the street.
- A variety of materials should be used including red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.
- Vertical windows with bay windows.



VF12 - Public Space:

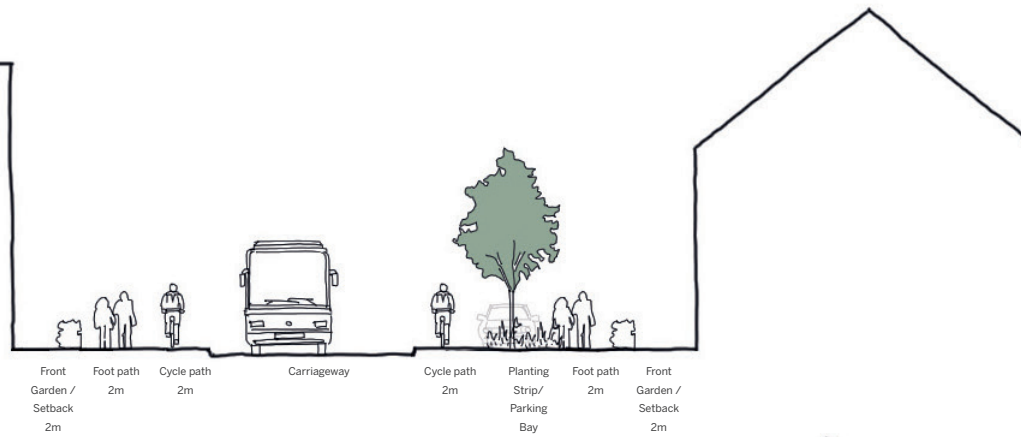
New public spaces should be designed to the specifications set out in the adjacent table.

VF13 - Mix of uses:

This area will be predominantly residential; however, home working should be encouraged enclosing studio spaces in gardens.

| VF12 - Public Space | | | | | | | Street Trees | Tree Spacing |
|---------------------|-----------------|-------------------------|--------------------------|--------------------|------------------|------------------------|----------------------|--------------------|
| | Enclosure ratio | Width between buildings | Building line compliance | Parking | Cycling | Footway | | |
| High Street | 1:2.5 | 16-20m | 75% | One side of street | 2m on both sides | At least 2m | One side alternating | Min 12m Max 24m |
| Secondary Street | 1:2 | 14-17m | 65% | Both sides | On Carriageway | At least 2m | Both sides | Min 12m Max 24m |
| Tertiary Street | 1.2 | 13-15m | 65% | Both sides | On Carriageway | Part of shared surface | Both sides | Min 12m Max 24m |
| Parkside Street | NA | NA | 75% | One side of street | On Carriageway | At least 2m | One side | Min 12m Max 24m |

Village Fringe
High Street



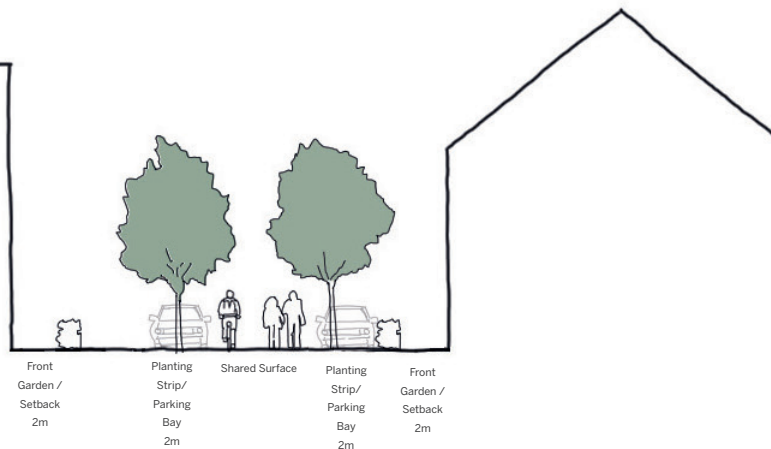
Horsted Park. gable ended frontages

Village Fringe
Secondary Street



Old Lodge Farm - variation roof and articulation of front facade

Village Fringe
Tertiary Street



Great, Knighton - tertiary street

3.2 Area Type 3: Rural Edge

Location

This area type covers the area to the east of the new village area and includes the northern and southern edges of the existing village.

The area includes high streets, secondary, tertiary and parkside streets, though the former will not include any shops.

Character

The character of the area is based on an analysis of nearby semi-rural settlements like St Mary's Hoo. The area is more rural in nature, and this is reflected in the following characteristics:

- Housing densities of between 25 and 35 dwellings per hectare.
- Predominant heights of 2 storeys.
- Buildings that are generally semi-detached, detached or bungalows.
- Buildings that informally follow a building line.
- Buildings that are set back 5-7m from the back of pavement.



Rural Edge Area Location

Rural Edge Coding

The following design codes set out the regulatory rules for the Rural Edge area, which development proposals must adhere to.

The subsequent pages present illustrations, sections and precedents that show how the design codes create the character of the Rural Edge.

RE1 - Streets:

Development in the village centre will define the Primary, secondary and tertiary streets shown on the plan on page 33.

RE2 - Cycle Parking:

To be provided based on 2 spaces per dwelling to be provided on-plot.

RE3 - Car Parking:

To be provided based on 2.5 spaces/ residential unit. On average, 2 spaces should be provided on-plot with the remaining being unallocated on street parking. The on-plot parking can be in front of the building line, but care should be taken to ensure that parked cars do not dominate the street scene.

RE4 – Green spaces:

The garden and courtyard spaces should be designed to maximise biodiversity.

RE5 – Housing Density:

New housing should be built at net densities of 25-35 dwellings / hectare.

RE6 – Party Wall:

Housing may be joined to their neighbours but are not required to do so.

RE7 - Form:

Development should create a varied and fine-grained development form with an informal layout and a range of materials.

RE8 - Building Line:

An informal building line should be set for development. This should be set 5-7m from the back of the pavement. Buildings should project forward and back from the building line by no more than 1.5m.

RE9 - Height:

The maximum building height should be 2 storeys with an eaves height not exceeding 7m. The total height of buildings should be no more than 3m above the eaves height and most buildings will have pitched roofs.

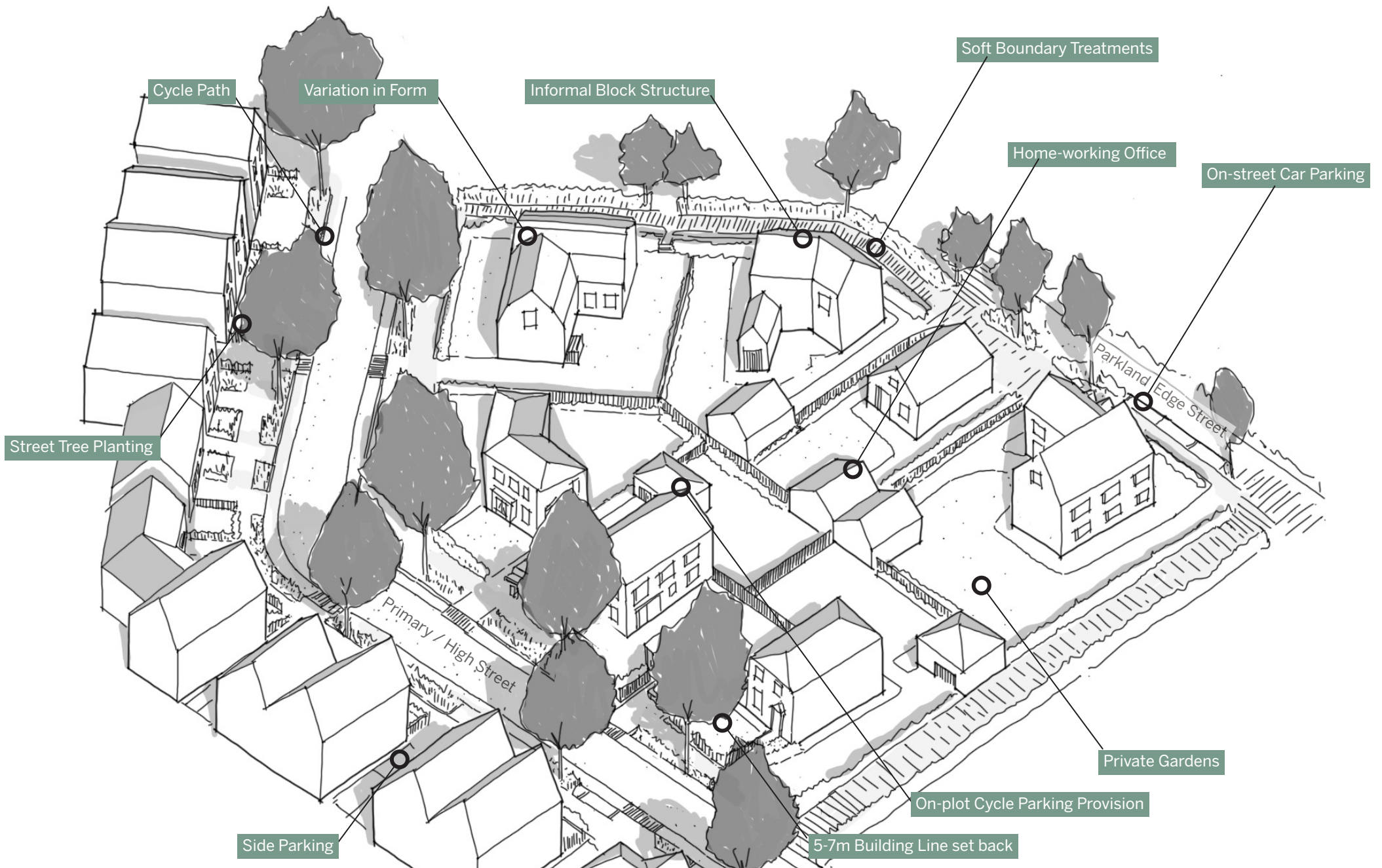
RE10 - Boundary Treatment:

The boundary of front gardens should be marked by a hedge or fence no more than 1m in height.

RE11 - Identity:

Buildings should be designed as a contemporary interpretation of local vernacular:

- Entrances should be marked with porches.
- Rooflines should be varied with some gables facing the street.
- A variety of materials should be used including red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.
- Vertical windows with bay windows.



RE12 - Public Space:

New public spaces should be designed to the specifications set out in the adjacent table.

RE13 - Mix of uses:

This area will be predominantly residential; however, home working should be encouraged enclosing studio spaces in gardens.

| RE12 - Public Space | | | | | | | Street Trees | Tree Spacing |
|---------------------|-----------------|-------------------------|--------------------------|-------------|------------------|------------------------|------------------------------------|--------------------|
| | Enclosure ratio | Width between buildings | Building line compliance | Parking | Cycling | Footway | | |
| High Streets | 1:4 | 23-30m | 60% | None | 2m on both sides | At least 2m | One side alternating | Min 12m Max 24m |
| Secondary Street | 1:3 | 20-24m | 50% | Occassional | On Carriageway | At least 2m | Both sides | Min 12m Max 24m |
| Tertiary Street | 1.3.5 | 18-20m | 50% | Occassional | On Carriageway | Part of shared surface | One side alternating or both sides | Min 12m Max 24m |
| Parkside Street | NA | NA | 60% | Occassional | On Carriageway | At least 2m | One side | Min 12m Max 24m |



St Mary's Hoo - Gable-ended garages



St Mary's Hoo, detached housing, wide frontages short set-back with white picket fence



Hall Road, St Mary's Hoo - short set backs, white picket fence and soft landscape boundary treatments

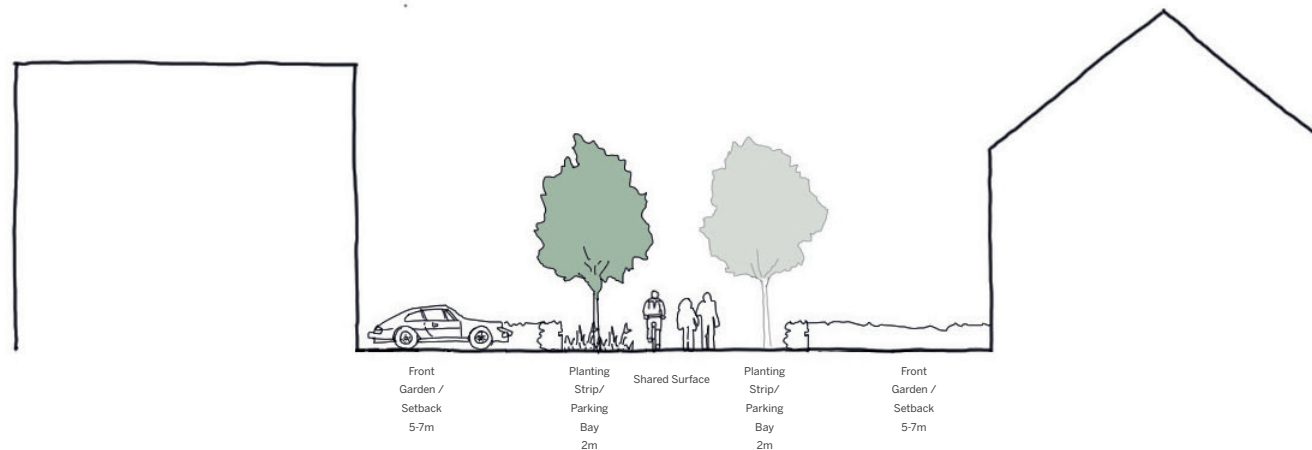
Rural Edge
High Street



Rural Edge
Secondary Street



Rural Edge
Tertiary Street



3.3 Area Type 4: Rural

Location

The Rural Area type applies to the remaining land area within the Neighbourhood Plan boundary but excludes the existing built settlement area of High Halstow, development along Sharnal Street (A228) and the land east of High Halstow.

Character

The existing character of this area is also identified and described within the High Halstow Historical Area Assessment. The area is predominantly made up of rural marshland, farmland and woodland. The northern third of the area is an expanse of low-lying reclaimed marsh, bisected by fleets and ditches.

The upland parts contain farmland that is arable, orchards and woodland, including the nature reserve at Northward Hill, Wybornes Wood and Fishers Wood.

Some several cottages and

outbuildings were used by sheep minders in the 19th century, with the only remaining example called 'Shade House' located north of Decoy Fleet, an enigmatic brick structure.

Several dispersed upland farmsteads have survived as well as their farmhouses without their outbuildings. A group of four listed farmhouses in the south and west of the parish was identified on the map.

The oldest of the four is Grade II listed Great Dalham on the Cooling Road, with a timber-framed structure clad with weatherboard. This building dates back to the early 16th century. Dux Court, Wybourne Farm and Buckhole Farm are also Grade II listed red-brick farmhouses that range from the late 17th century to the early 19th century.

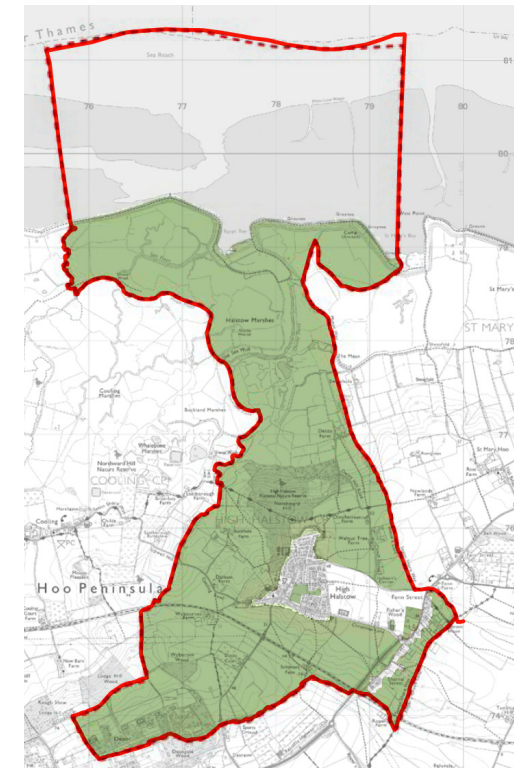
Other surviving buildings include Solomons Farm, south of the

mineral railway, and Decoy Farm to the north of Clinch Street; the latter may have replaced a farmstead associated with the decoy pond in the marshes, lost its farmhouse in the 1960s.

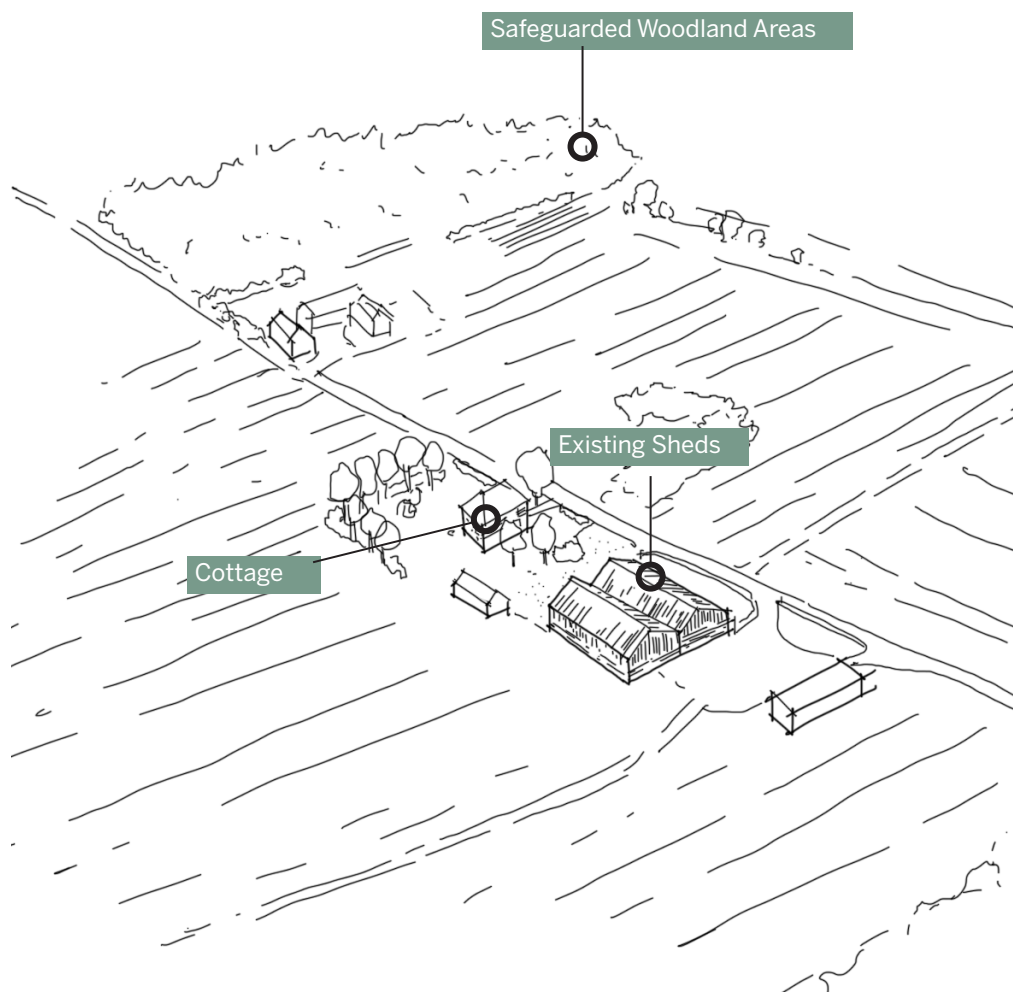
Two farms remain from the small cluster at Clinch Street, Clinchstreet Farm and Walnut Tree Farm.

Northward Hill is one of the higher points in the peninsular and is covered by a mixture of hawthorn scrub and ancient oak woodland.

This area has formed part of the High Halstow National Nature Reserve, managed by the Royal Society for the Protection of Birds. The nature reserve is an important educational and leisure asset for residents and visitors to the area.



Rural Area Type Location



Rural Area Type Character

Rural Coding

The following design codes set out the regulatory rules for the Rural area, which development proposals must adhere to.

R1 – Green spaces:

Any development proposals that would cause unacceptable harm to the local landscape character, archaeological or nature conservation will not be permitted.

R2 - Identity:

Any proposed extension should be designed as a contemporary interpretation of local vernacular:

- Entrances should be marked with porches
- Rooflines should be varied with some gables facing the street.
- A variety of materials should be used, including red, yellow and buff brick, weatherboarding in white and black, clay or slate roofs.
- Vertical windows with bay windows.



Decoy Farm - Sheds and Cottage



Walnut Tree Farm on Clinch Street



Shade House, High Halstow Marshes

